



# Land at Station Hill, Botley

## Vision Statement

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**LOCATION**

Land at Station Hill, Botley

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**HGP REFERENCE**

22.137

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**APPLICANT**

Foreman Homes

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**REVISION**

A - 12th June 2024

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**DATE**

31st May 2024

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**STATUS**

Site Promotion

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**AUTHOR**

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**REVIEWED BY**

HS Master Land and Planning

# 01 Introduction

Preface	Page 3
Project Team	Page 4
Development Brief	Page 5
Winchester Design Guidance	Page 6
Building for a Healthy Life	Page 7

# 02 Analysis

Existing Site	Page 9-13
Urban Analysis	Page 14-17
Site History	Page 18
Local Development	Page 19-21
Site Constraints and Opportunities	Page 22-23
Local Character	Page 24-26
Architectural Details to Bring Forward	Page 27
Landscape Study	Page 28
Flood Risk and Drainage	Page 29

# 03 Illustrative Masterplan

Constraints Diagram	Page 31
Concept Plan	Page 32
Annotated Illustrative Masterplan	Page 33
Figure Ground Diagrams	Page 34
Access & Movement	Page 35
Streetscape Strategy	Page 36
Illustrative Masterplan	Page 37

# 04 Architectural Character

Precedents	Page 39
------------	---------

# 05 Conclusion

	Page 40
--	---------

# 01

## Introduction

## Preface

This document has been prepared by HGP Architects on behalf of Foreman Homes to support the promotion of Land at Station Hill, Botley for a residential development of up to 177 homes.

The site is located to the south of the Winchester District, east of Eastleigh Borough. It has a long history of agricultural uses such as cattle grazing (although the site has also been used to host car-boot sales), and it is primarily accessed from the A3051.

### SITE ADDRESS

Land at Station Hill, Botley Road  
Curdridge  
Southampton  
SO30 2HD

### SITE AREA

11.77 Ha

### LEGEND

Site



Location of Site  
Approx Site Area: 11.77Ha

## Project Team

### CLIENT



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## Development Brief

### THE VISION

Foreman Homes are committed to providing high quality, affordable homes for all. In this semi-rural (as a result of recent development and approvals) location, there is an opportunity to provide a landscape-led development that utilises successful urban planning to create a beautiful place.

We envision slow streets that prioritise pedestrians and cyclists, green avenues, pleasant cul-de-sacs, and a destination public open space.

### THE NUMBERS

Site Area: 11.77Ha

Site Area Minus Approved Bypass Road: 11.20Ha

Approximate Developable Area: 5.9Ha

Target Density: 30dph

Total Number of Units: 177 Units



## High Quality Places SPD

Available to download from the Winchester City Council Website are a variety of Supplementary Planning Documents that provide further details and guidance on Local Plan Policies. The High Quality Places SPD was adopted in 2015 and provides comprehensive guidance that helps designers to create places that are successful and appropriate for the local area.





## Building for a Healthy Life

Building for a Healthy Life is a design toolkit for neighbourhoods, streets, homes, and public spaces. The document has been designed to allow a wide range of people to use it easily, providing a large number of examples of both good and bad design.

The three key headings within the document are:

- Integrated Neighbourhoods
- Distinctive Places
- Streets for All

By following the guidance in this document, designers can avoid common pitfalls and improve the overall quality of the places that are created through development.

This document has been referenced throughout the design process for the site.

## Streets for a Healthy Life

Streets for a Healthy Life is a companion guide to Building for a Healthy Life and expands on the previously mentioned guidance. It is being continually expanded with new street typologies and technical details over time, allowing the document to evolve as England's streets evolve.

The document makes reference to the guidance provided in Manual for Streets and helps designers to create legible and attractive streets. The document contains a range of case studies to help designers understand how small design choices can lead to positive or negative spaces.

This document has been referenced throughout the design process for the site.

Table 2: Typical Street Features

Street Typology	Features									
	Place		Movement				Access	Parking	Drainage, utilities & street lighting	
	Mixed Use	Street Trees / Landscape	Footways	Protected Space For Cycling	Public Transport	Shared Surface	Direct Car Access	Car Parking Bays	SuDS	Street Lighting
Principal / Main Street	Typically	Typically	Always	Typically	Typically	Rarely	Typically	Typically	Typically	Typically
Secondary Street	Typically	Typically	Always	Typically	Typically	Rarely	Typically	Typically	Typically	Typically
Tertiary Street	Typically	Typically	Typically	Rarely	Rarely	Typically	Typically	Typically	Typically	Typically
Edge Lane / Private Driveway	Rarely	Typically	Typically	Rarely	Rarely	Typically	Typically	Always	Typically	Typically

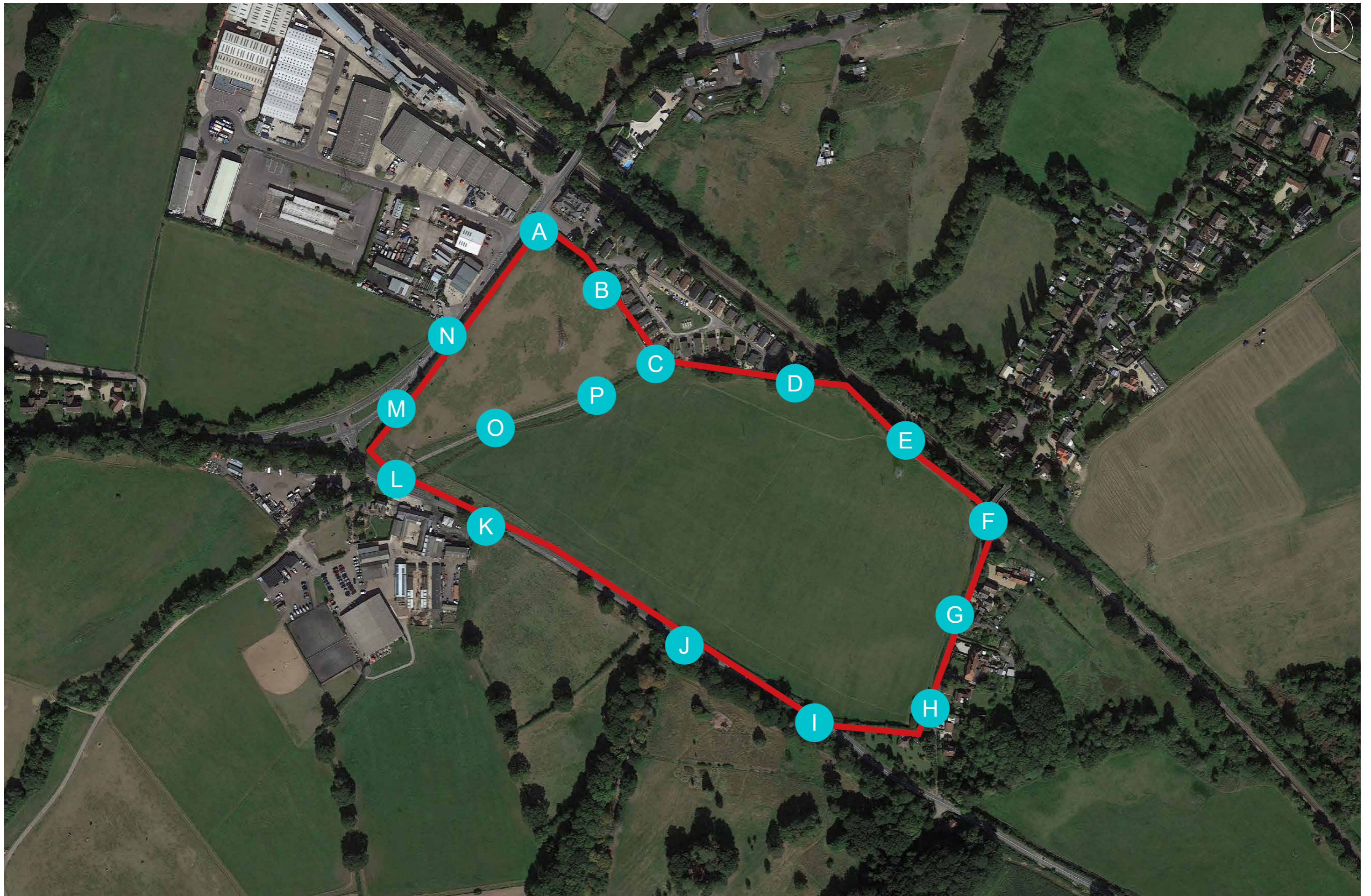
- Always
- Typically
- Sometimes
- Rarely

# 02

## Analysis



Photograph of Site from Public Right of Way (close to point A)



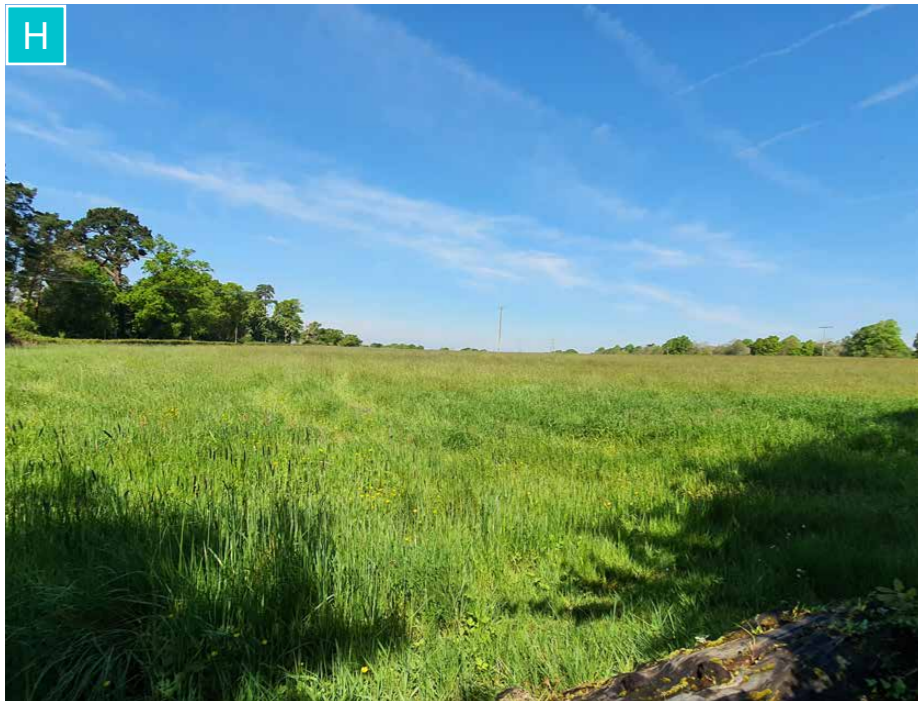
Site Photographs Key

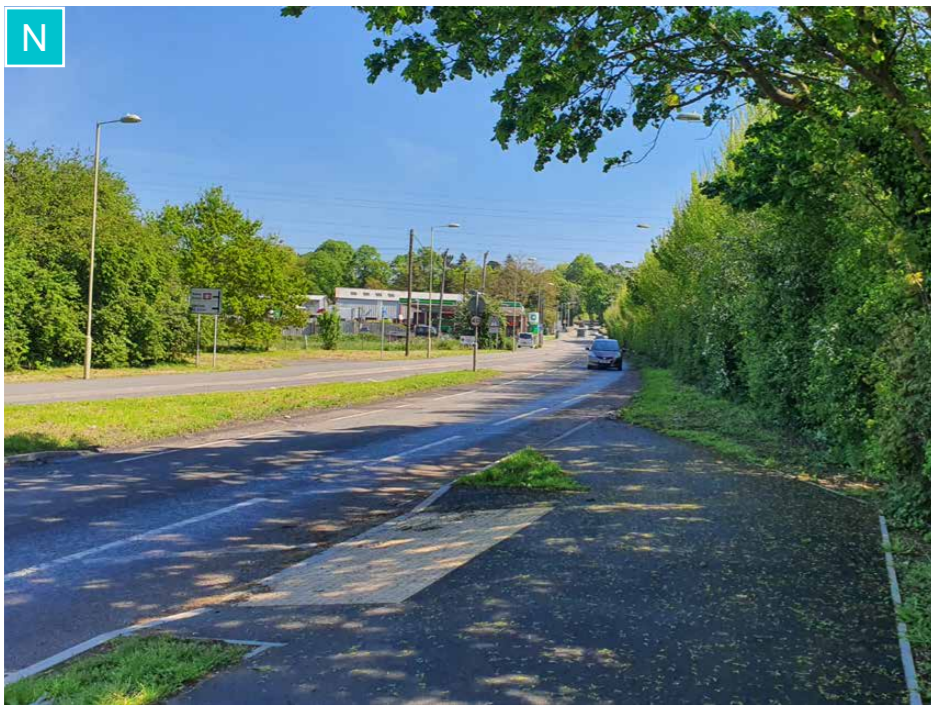
## Site Photographs

The site is currently a large arable field located between Botley Station and Outlands Lane.

Vehicular access to the site is either via the newly formalised access from the A3051 or the older, more secondary vehicular access from Outlands Lane. The site has two public right of ways running through for easy pedestrian access: one running north-west to south-east from opposite Botley station through to Outlands Lane, and the second running alongside the stream from the new vehicular access from the A3051 until it intersects with the first public right of way along the northern boundary of the site.







## Urban Analysis

The site is located opposite Botley train station, 600m to the east of Botley High Street. Two bus stops immediately border the site, allowing easy access to buses 49, 649, S2, 28, and 28a for transport to Swanmore Technology College, Bishop's Waltham, Hedge End, Whiteley, Fareham, and further afield. There is a newly implemented multi-user pathway to the south of the site that provides a cycle route towards Botley or Whiteley from the site.

There are two primary schools within easy walking distance: Curdridge Primary School to the north and Botley C of E Primary School to the east.

The site has good vehicular links via the A334 and A3051 leading to Junctions 7, 8, and 9 on the M27, providing motorway links towards both Portsmouth and Southampton. Whiteley Shopping Centre is a 10 minute drive to the south of the site (approximately 3miles or 4.8km).

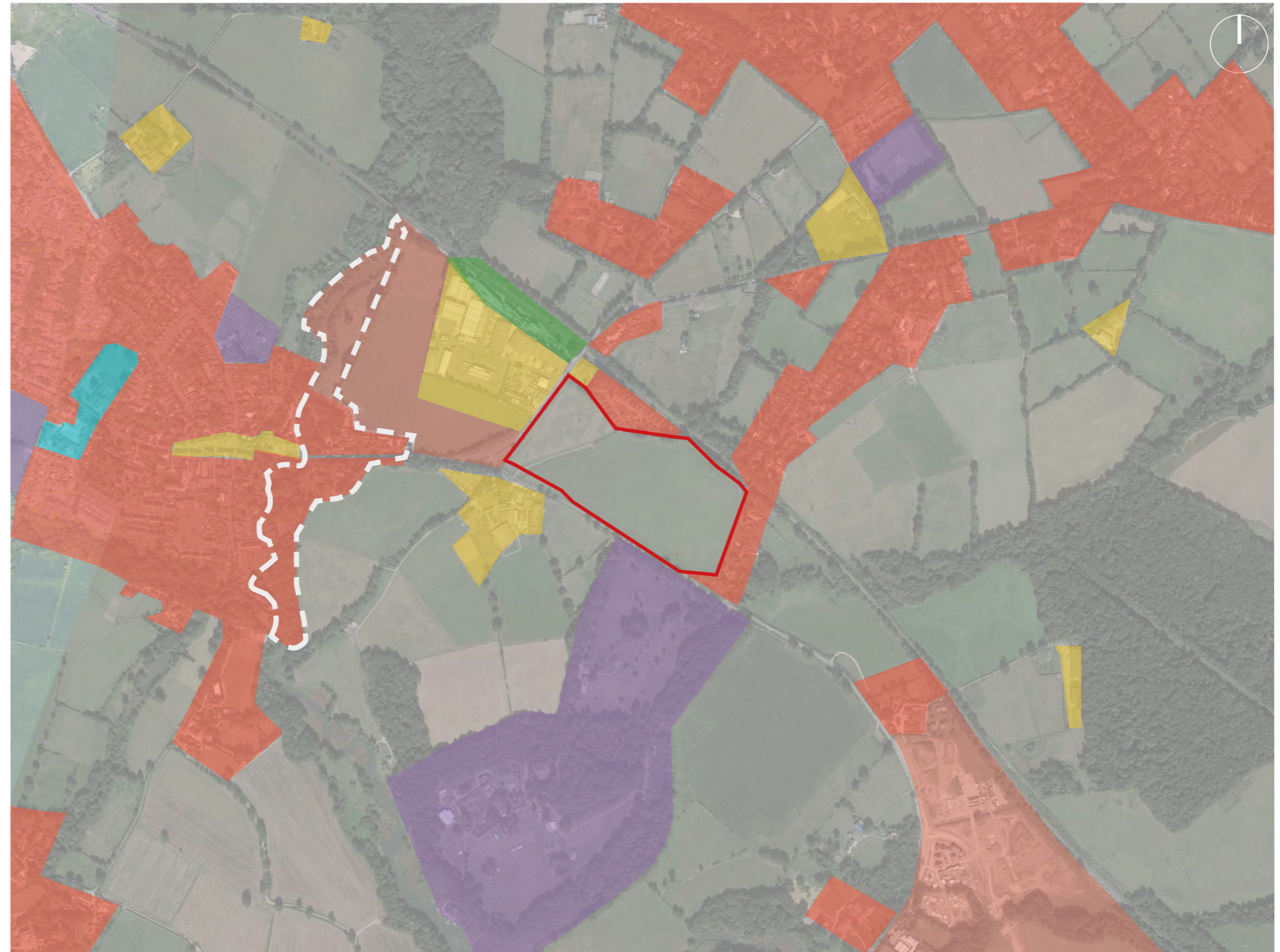




Approximate walking distances to local amenity and places of interest around the site

### Surrounding Use & The Community

The site primarily borders residential areas, including areas allocated for residential under construction. Located opposite Botley Station and Botlings Estate, the site is well-located for easy public transportation and benefits from links to employment.



Existing Use Diagram

**LEGEND**

- Site Boundary
- Residential
- Residential Approved/Under Construction
- Education/Health
- Commercial/Industrial
- Community
- Transport
- Botley Conservation Area Boundary

## Reading the Local Grain

The figure ground plan to the right shows how the settlement of Botley is naturally expanding with built form following organic street patterns. South of the site, buildings are mainly found in more agricultural clusters that have been built independently of any village.

East of the site, the proposed Sherecroft Farm Development (20/00494/FUL) naturally bridges the gap between Botley Village and the site, making Land at Station Hill a natural extension to the surrounding residential areas.



Figure Ground Diagram - Site as Existing

### LEGEND

- Site Boundary
- Existing Built Form
- Built Form Under Construction/Approved

## Site History

The settlement of Botley grew up around their original asset, the lowest crossing point of the River Hamble. The Square situated on the High Street was designed as a market place ideally located at the junction of the roads from Fareham, Bishops Waltham, Southampton and Winchester.

Since 1950, new homes houses have been built on three sides of the village; new estates were built between Winchester Street and Holmesland Road to the north of High Street; Mortimer Road and Four Acre to the south of the High Street followed by the development of Hamblewood between Church Lane and the river with more recently Hamblewood Court and The Quay off Church Lane to the south with Warner Mews and Reeves Rowland Court off the northern side of The Square.

Further development expanding Botley centre has been approved, and include the bypass alleviating the vehicular traffic through the centre of the town, a scheme for 112 homes adjacent to Botley Station, 375 houses north of the station, and a further 600 houses at the beginning of the bypass. The final stage of the bypass is a new roundabout and link into the A3051 just south of the site.

The site itself has been primarily used for agricultural purposes for over 100 years, with the public rights of way present in OS maps as old as 1909.



Historical Map published 1909 showing the footpaths through the site

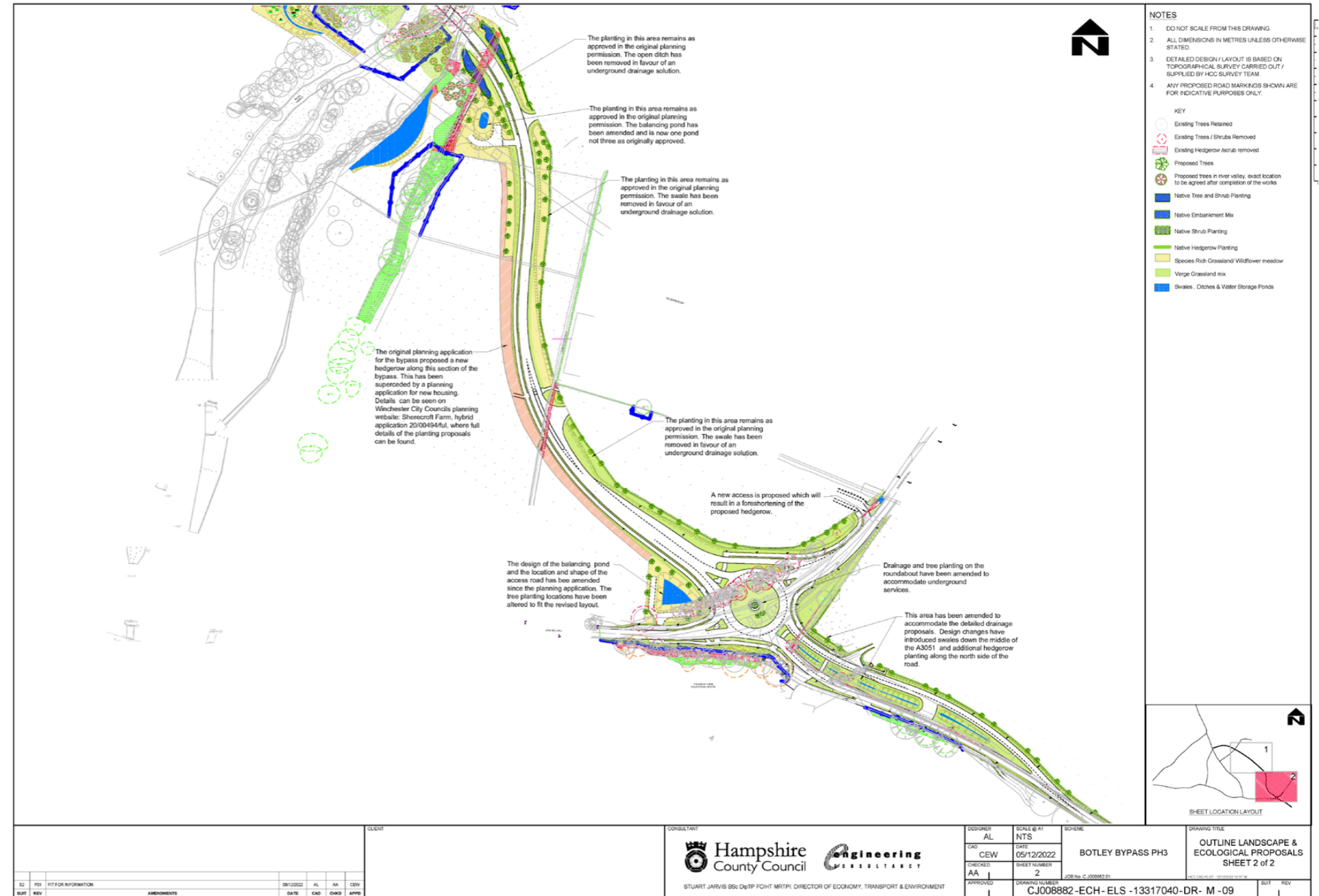


Aerial photograph from circa 1947 showing the division of fields around the footpaths

## Botley Bypass

Hampshire County Council is in the process of building a new Bypass to relieve traffic congestion and improve air quality in Botley Village. This Bypass creates a new roundabout that supports its connection into the A334 and A3051 before terminating just south of the site. At the time of writing this document, this work is due to be completed in Spring 2025.

West of the A334, a hybrid planning application (20/00494/FUL) has been approved for 0.88Ha of mixed use commercial (B1, B2, and B8), including employment development and D1 healthcare use (outline), a new station car park (full application), and 115 dwellings (full application) that runs along the western side of the bypass facilitating its construction.



Drawing Submitted to Winchester City Council in support of application 23/01327/HCS | Partially Discharge conditions 3, 10, 15 & 16 of 17/02023/HCS and 21/02012/HCS Variation of Condition 15 to provide for revised Arboricultural Impact Assessment and Arboricultural Method Statement | Proposed Botley Bypass Botley Hill Botley Hampshire

## Sherecroft Farm Development

The 112 new dwellings approved east of the site (20/00494/FUL) is defined by a series of character spaces. Each area reads as a family of buildings with matching properties to define those spaces, and all units have been heavily inspired by details featured on the built form within the area. House types, window modules, bays, roof pitches etc. are constant throughout the site, giving unity to the whole, but each area has its own defining properties to these ingredients, which set it apart from the next space.

The 7 character areas are:

- The Crescent
- Woodland Perimeter
- Eastern Perimeter
- Courtyards
- The Paddock
- Farmyard
- Employment

The approved site plan is shown to the right, with a selection of the application's 3D views shown on the following page.



Extract from Approved Site Plan: 19.032.100 Proposed Roof Layout



Crescent Character Area



Crescent Character Area



Courtyard Character Area



Woodland Perimeter Character Area



By Eastern Perimeter Character Area



By Eastern Perimeter Character Area

## Site Constraints

The site comprises two large fields separated by a stream and a public right of way. To the north of the site is a mobile home park known as Woodview Park that has back gardens facing the site. These back gardens are mostly fenced with gates allowing access to the public right of way. Some trees along this northern boundary fall into TPO Area 0602A1.

Just to the north of the site, on Station Hill, there are three Grade II listed buildings: a drinking fountain, monument and a stone.












To the northeast, railway tracks run parallel to the site, although significantly lower than the levels on site.

Several constraints run through the site, such as a stream, overhead electrical cables, electrical pylons, an existing sewer and two public rights of way.

A cycle route and multi-user pathway runs across the south-western corner of the site, crosses the road, and continues to run southeast towards Whiteley.

The Botley Bypass is currently under construction, with the final phase estimated to be completed in Spring 2025. The final phase utilises 0.57Ha of the land in the south-western corner of the site to join into a new roundabout.

### LEGEND

-  Site Boundary
-  Existing Access
-  Stream and Flood Risk Area
-  TPO Area
-  Overhead Cables
-  Electrical Pylons and Cables
-  Existing Sewer
-  Existing Water Pipe
-  Public Right of Way
-  Potential noise source
-  Location of Listed Buildings



Constraints Diagram



### Site Opportunities

Putting landscape at the forefront of this scheme, the site has the opportunity to enhance the landscape surrounding the public rights of way through new public open space, and provide new green corridors for wildlife and ecological enhancement.



Opportunities Diagram

**LEGEND**

-  Site Boundary
-  Developable Area
-  Vehicular Access
-  Potential Emergency Access
-  Suggested Public Open Space
-  Potential New Green Link/Corridor
-  Public Right of Way

## Local Character - Architecture

### BOTLEY

This historic village of Botley initially developed as a natural crossing point for the River Hamble, and received its first market charter from Henry III in 1267. The village grew on the success of its coaching inns and mill, which is still a prominent feature along the A334, although it has now been converted into a restaurant and series of small shops.

Botley's central core is only 15 minutes walk from the site, and still features a range of two storey houses from a variety of ages and of varying grandeur. These houses primarily feature pitched roofs with chimneys and red/brown clay tiles or natural slates, facing brick walls with a predominance of red brick and burnt end detailing. Some of these houses also feature tile hanging and/or white painted brick or render.



Botley Mills Panorama Photograph



Botley Mills Converted Building Facing Mill Hill/A334



Historic Terraced Housing on Mill Hill/A334



Georgian house with grey brick front facade, burnt-end brick side façades, skewback arch brick headers, and larger windows to ground floor.



Detached house with recessed porch, symmetrical ground floor bay windows, and skewback arch stone headers.



Semi-detached house with double front gable detail, tile hanging to gables, arched brick headers, and dogtooth course brick detail.



Detached Chalet Bungalow with tile hanging to first floor, and timber-coloured windows that match the front door.



Georgian-Style Houses with parapet roof, dentil course brick detail, and stone-effect window surrounds.



Farmyard-Style Terraced Houses with dentil course brick detail, chimneys, and flat canopies to front doors.

## Local Character - Architecture

### OUTLANDS LANE (SOUTH OF THE RAILWAY)

A section of Outlands Lane immediately borders the eastern edge of the site. It features a variety of different styles of architecture, with each house different from its neighbour. The lane primarily features detached houses, although there is one large terraced block of three houses in the centre.

Key architectural features include:

- Brown roof tiles
- Red brick façades
- Chimneys
- Feature gables
- Large white-framed windows
- Built-Out Porches



Semi-Detached House with both tile hanging and timber boarding to the front facade.



Terraced Houses with mock-Tudor boarding, curved brick headers, and integral garages.



Detached House with pebbledash to first floor, chimneys, and built-out glass-roofed porch.



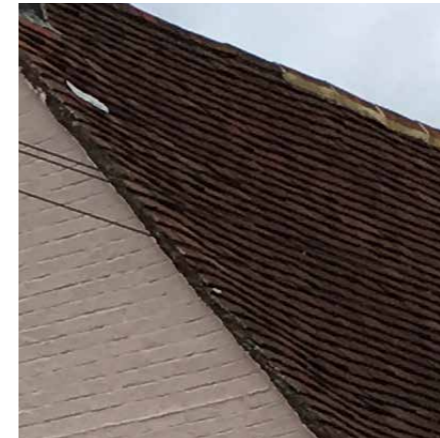
Detached Chalet Bungalow with built-out gable porch and symmetrical ground floor bays on A3051, opposite the southern end of Outlands Lane.

### Architectural Details to Bring Forward

The adjacent images show a selection of materials and architectural details that have been extracted from houses in the local area. A selection of these materials, colours, and details could be brought forward when considering the design of the houses on site to ensure the proposal is in keeping with the existing local vernacular.



Hipped Roofs in Brown Tile



Front-Back Pitched Roofs in Brown Tile



Feature Gables



Chimneys



Flemish Bond in Burnt-End Brick



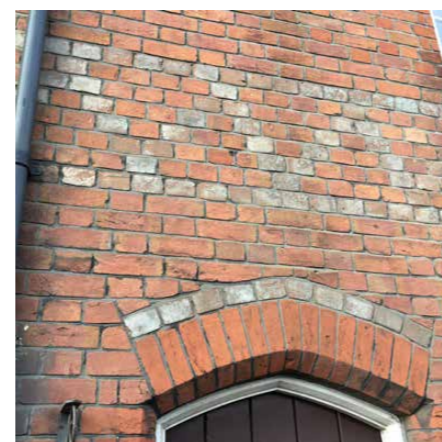
White Painted Brick



Brick Dentil Course Detail



Brick Feature Detailing



V-Shaped Repeating Detail in Contrast Brick, Soldier Course Arched Headers



Red Brick Skewback Arch Headers, Portrait Georgian-Bar Windows



Landscape Georgian-Bar Windows



Flat-roofed Canopies to Front Doors

## Landscape and Visual Analysis

A landscape study has been prepared to assess the capacity of the site for residential development, and to identify a series of masterplan design principles to respond sensitively to the landscape and visual characteristics of the Site and its context.

The site is not subject to any landscape related designations, albeit listed buildings and ancient woodland are present in the local area. It is located within a Landscape Character Area (LCA) which is noted as comprising gently undulating lowlands of poor agricultural quality with streams, and a predominance of woodland and strong hedgerow field boundaries resulting in a sense of enclosure with occasional long views. The northern part of the LCA, where the site is located, is described as having 'a far less wooded and more settled character' to the south.

An intermittent framework of boundary trees and hedgerows creates a strong sense of enclosure in lower lying parts of the site, however to the north-east, elevated landform allows more distant views across the River Hamble valley.

The site is visible in open views from the Public Rights of Way that extend across its interior, while boundary hedgerows partially filter views from adjacent roads to the south and east, including from existing houses. To the west, a robust hedgerow strongly contains the site, with gaps to the north and south. The northern boundary is partially enclosed by woodland along the railway line, albeit there are open views from the adjacent residential park. All of the available views are within, or in close proximity to the Site, with strongly vegetated Site boundaries providing substantial screening of the Site's open interior. Further afield, visibility diminishes rapidly as a result of a combination of landform and the strong wider framework of vegetation.

The Site is considered to have the capacity in landscape terms to accommodate residential development, subject to a sensitive landscape-led design approach. The following landscape and visual design principles have been identified to inform the emerging masterplan for the proposed development:

- Retain and reinforce existing boundary hedgerows and trees, including strengthening the south-eastern boundary to address views from existing houses;
- Provide a landscape buffer on the northern boundary to address views from Woodview Park;
- Provide a Green and Blue Infrastructure corridor following the course of the watercourse and PRow;
- Provide a sensitively designed landscape buffer to the existing high voltage overhead power lines;
- Create a wetland landscape in the Site's south-western corner, forming a bookend to the watercourse landscape corridor;

Further details can be found in statement 24.1942 "Landscape and Visual Study" submitted alongside this Vision Statement.



**LEGEND**

- Site Boundary
- Public Rights of Way
- Retained and Reinforced Vegetation
- Views from Neighbouring Houses
- View Across River Hamble Valley
- Public Open Space under Overhead Power Line
- Green Blue Infrastructure Corridor
- SUDS Zone / Features
- Development Parcel (approximate 5.78 hectares)
- Key Destination / Gateway
- Potential Green Infrastructure Link to Wider Landscape Pattern
- Opportunity to Provide Visual / Pedestrian Connection with Railway Station / Pub
- Proposed Boundary Planting as Part of Proposed Botley Bypass
- Potential Site Access (to be confirmed by highway engineer)
- Extent of Proposed Botley Bypass (Refer to Winchester Local Plan 2019-2039, Policy SH6)

Rev	Date	Amendment	Initials

Project: STATION HILL, BOTLEY

Client: FOREMAN HOMES LTD

Drawing: LANDSCAPE DEVELOPMENT PRINCIPLES PLAN

Drawing no: 24.1942.L001 Rev: -

Scale@A3: 1:2500 Date: May 2024 Drawn: CLB Checked: JM

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PRELIMINARY



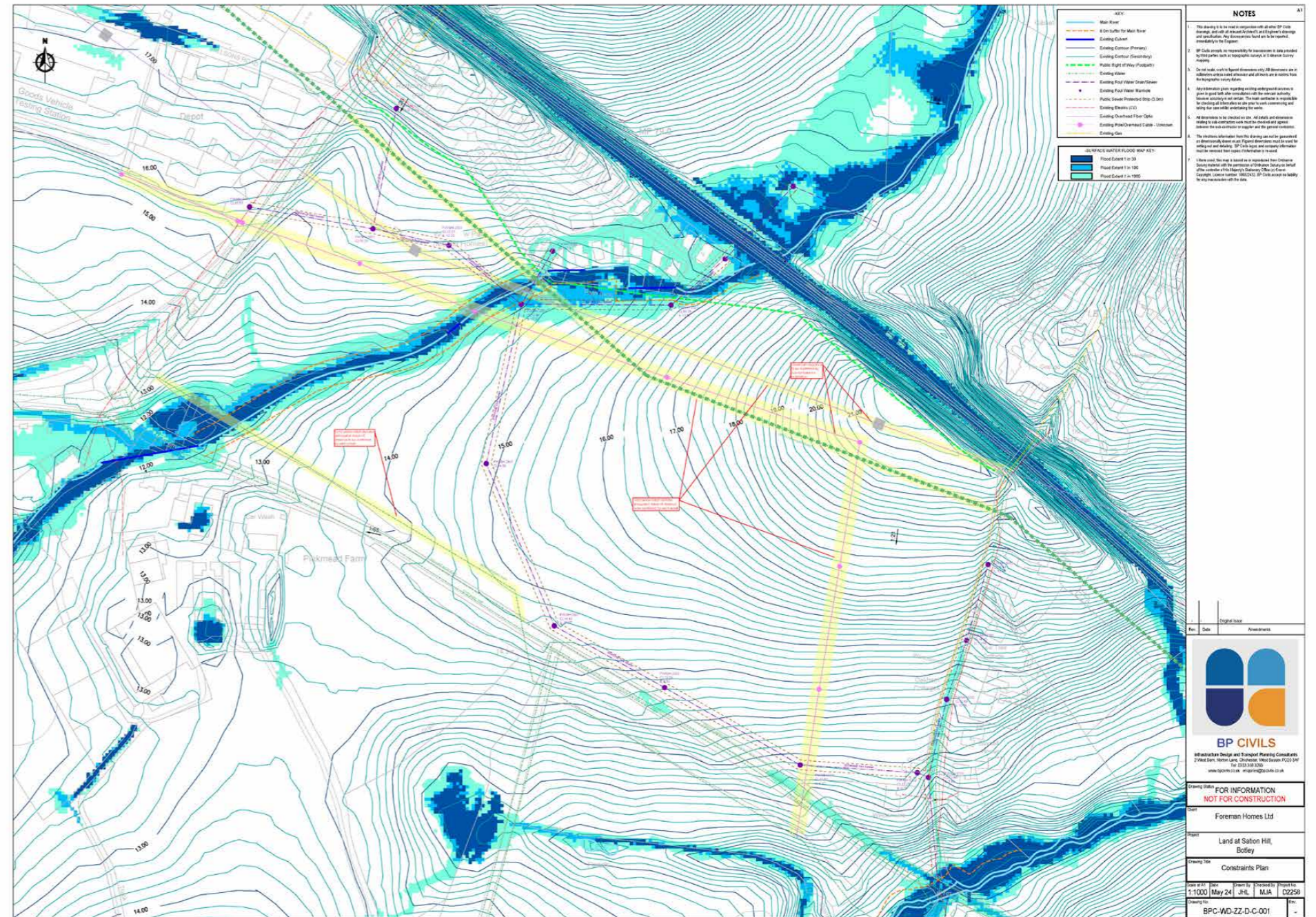
Appendix A from the Landscape and Visual Study by FINC Architects

## Flood Risk and Drainage

While the site is situated within Flood Zone 1 (lowest risk) a small section of the site is at risk from surface water flooding due to the watercourse running approximately southeast to southwest through the site. As a result, any proposed built form should be a minimum of 8m from the watercourse.

There are several existing pipes running through the site, both foul water and water supply, and a review of the Southern Water sewer records indicate a gravity connection to the public foul sewer that runs through the site is viable.

Further details on flood risk and drainage can be found in report D2258 Flood Risk and Drainage Appraisal by Bright Plan Civils, submitted alongside this Vision Statement.

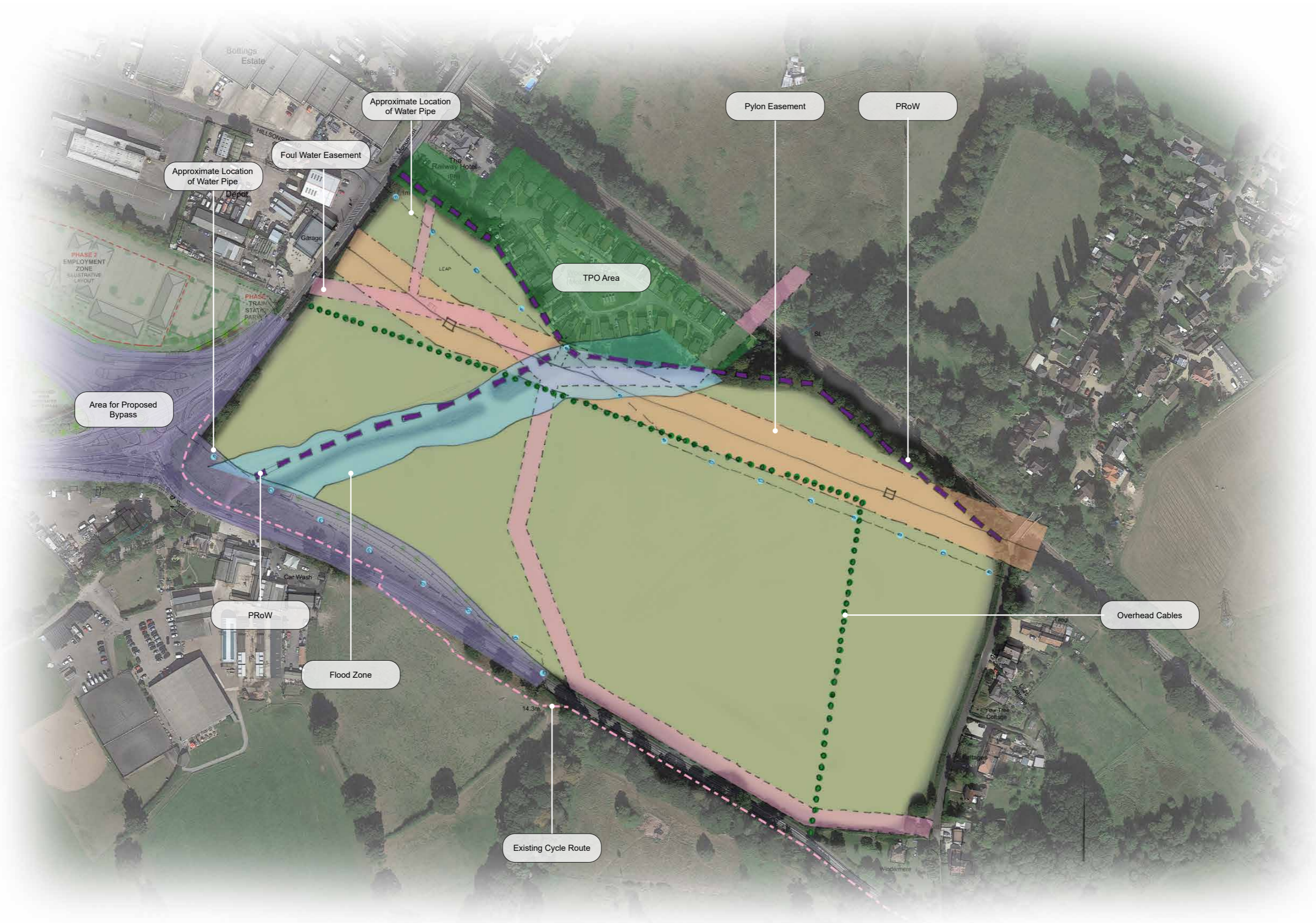


Drawing number BPC-WD-ZZ-D-C-001 by Bright Plan Civils

# 03

## Illustrative Masterplan





Constraints Diagram

## Concept Plan

The concept for the site has been designed around providing a large public open space to the north of the site, and two new green corridors crossing the site that run alongside the site accesses. The aim is to provide a visual link to nature throughout the site, and ensure that Land at Station Hill provides a wonderful place to live.

A new access is proposed from the A3051, with the public open space placed as an end-of-vista visible from the site entrance. The main roads throughout the site each end in a green view, whether that be of the public open space or the enhanced planting to the site borders.

The existing access from the A3051 is being retained and improved as part of the new Botley bypass works, but is the width of a single carriageway. This access could be used as a secondary or emergency only access into the site (left-hand turn only) to provide additional permeability into the site.

The pedestrian accesses to the Public Right of Way have been retained at the eastern and western edges of the site, and it would be beneficial for these to be upgraded and integrated within the new footpath network for the site.

Blue infrastructure such as SuDs features have been placed at the lowest points on site, with clear views from the accesses on site, providing visual interest and biodiversity opportunities.

The existing pylons have been retained, and an appropriate easement has been allowed between the pylons and developable area.

### LEGEND

- Developable Area
- Green Corridors
- Blue Infrastructure
- LEAP
- Primary Road
- Secondary Road
- Existing Public Right of Way
- Existing Cycle Route
- Proposed Vehicular Access
- Proposed Pedestrian/Cyclist Access



Concept Plan

## Illustrative Masterplan

The illustrative masterplan refines the ideas outlined in the concept plan, and suggests how these ideas could be translated into a site layout. The parcels of developable area shown in the concept plan have been divided into organic shapes, mimicking the local grain identified in Botley.

Green buffers have been enhanced, a new green corridor introduced to link the surrounding green infrastructure, and all public rights of way have been placed in public open spaces. The main road from the site access has been envisioned as a formal tree-lined avenue, with green connections continuing east-west along the public open space.

Nature has been integrated into the streets, with plenty of space for hedging, tree planting, and green verges. Long distance views terminating in dwellings or green features such as public open space, enhancing wayfinding throughout the scheme.



Illustrative Masterplan

### LEGEND

- · - Existing Public Right of Way
- · - Existing Cycle Route
- ↔ Proposed Vehicular Access
- ↔ Proposed Pedestrian/Cyclist Access

Figure-Ground Comparison



**FIGURE-GROUND - AS EXISTING**

The site currently features no built form, other than two large pylons that are not illustrated in the figure-ground diagram.



**FIGURE-GROUND - PROPOSAL**

The illustrative masterplan mimics the urban grain of both the nearby new development and Botley. While the majority of the new dwellings have been grouped together to create a variety of street patterns, small sections of development are arranged in "farmyard clusters" to mimic local smallholdings.

## Access & Movement

The masterplan has been designed to maximise pedestrian connectivity throughout the development, with plentiful links to the public rights of way and the surrounding roads.

The road hierarchy has been designed to promote easy navigation around the site with wider, more formal roads, softening down into smaller shared surfaces towards the edges and courtyards. Footpaths should be provided alongside the spine road and primary roads, while secondary and tertiary roads either feature a footpath along one side or are shared surfaces that prioritise cyclists and pedestrians.

Public open space facilitates a range of routes across the site for pedestrians and cyclists, encouraging local residents to travel sustainably and experience nature. The LEAP has been positioned to allow easy access to residents and those walking along the public right of way.



Access and Movement Diagram

### LEGEND

- Primary Road
- Secondary Road
- Tertiary Road
- - - Pedestrian/Cyclist Connection
- ▲ Main Site Access

### Streetscape Strategy

The streets have been designed with a range of precedents in mind. The images to the right show a selection of precedents that have been used when designing the site layout, and demonstrate how the methods shown on the masterplan can be successful.



Driveway Parking Precedent: Parking kept out of the street scene between houses.



Tertiary Street Precedent: Service strip and road in the same paving material encourages slow vehicular speeds and pedestrian priority.



Green Avenue Precedent featuring street trees and lay-by parking spaces surrounded by planting.



Boundaries Precedent: Planting strips alongside closeboard fences to soften the street scene



Illustrative Masterplan showing PRoWs and Existing Cycle Route

# 04

## Architectural Character



## Precedents

The architectural character of this site should draw strongly from the historic character of Botley and upcoming neighbouring developments. Homes close to Outlands Lane should primarily reference the houses on Outlands Lane, providing opportunities for multiple character areas on site that enhance and strengthen the masterplan's design.

There is an opportunity to use a variety of materials on site to create character areas within the development. These character areas could be similar in overall form, but utilise different facade materials and detailing to help residents and visitors navigate through the scheme. Character areas will help promote variety in the architecture across the site, aiding the development of an exciting place to live.



Neighbouring Sherecroft Farm Development



Local Detached House



Local Terraced Housing



Local Terraced Housing

## An Opportunity for Place-making

The masterplan for Land at Station Hill has been designed to connect and enhance green spaces. New green corridors provide not only visual benefits to the scheme, but also ecological benefits that promote biodiversity. Buildings have been located to provide interest and promote wayfinding, and when utilised alongside streets that prioritise pedestrian and cyclist movement and architecture that references the beautiful local history, Land at Station Hill will become a lovely place to wander and admire.

Foreman Homes and Master Land and Planning are working with HGP Architects to review any additional constraints and analysis required. Following any further information from additional consultants, HGP will work alongside these additional consultants to refine the masterplan for the site. Together, our goal is to help the Station Hill masterplan be the best that it can be, providing homes and places that people are proud of.

Thank you